



Coast Meet – September 25, 2022 SBRHS, Clinics, and Open Layouts

Updated 8/26/22

Coast Meet – SBRHS, Clinics, Open Layouts – September 25th 2022

The next Coast Meet event will be held on September 25th at the South Bay Railroad Historical Society in Santa Clara. This event will feature:

- 4 great clinics in the morning
- Visit to the SBRHS facility and their layout
- Lunch at a local restaurant
- 8 or more open layouts in the afternoon

Bookmark this [link](#) for a pdf of this Meet to view on your phone.

Vaccinations and Masks

At SBRHS

It is requested that attendees be vaccinated, masks are recommended.

Coast Member Layouts

For the Coast member layouts that are open, it is left to the members discretion whether to limit entrance to vaccinated individuals and to require masks. Please respect their wishes as they open their homes to us. Carrying proof of vaccination with you is always a good idea. The open layout address handouts distributed at the SBRHS will have more information.

Schedule

The schedule for the day is:

9:30 AM – Meet Opens

10:00 AM and 11:00 AM We will have four clinics – details below

Lunch Lunch will be individual at:

Mission City Grill
2000 El Camino Real Ste 1
Santa Clara, CA 95050.

1 – 5 PM Members Layout Open Houses -Address to be distributed at SBRHS

If a member cannot go to the morning meet but is interested in going to the open layouts, please reply for an address list. Addresses will not be sent generally or to non-NMRA members. If you have a non-member to invite, have them come to the meet at the SBRHS and introduce themselves. If you are attending and have issues, my mobile is 408-832-5618.

Clinics

Earl Girbovan

Scenery Techniques and 2022 National Convention Review

This clinic will feature Earl discussing new scenery techniques he has developed, followed by a discussion of the 2022 NMRA National Convention with photos that Earl took while he was there.

Dave Gibbons

Hands on with Soldering

David will conduct a clinic on soldering fundamentals and techniques. This clinic will include the basics of soldering, irons, flux, types of solder and more. The clinic will include multiple stations for attendees to try the techniques and for their own soldering.

Dave Adams

Scratchbuilding

Dave will do a clinic on how he scratchbuilds amazing structures for his On3 layout. The clinic will include design and building techniques that can be used to build outstanding models.

Fran Foley

Introduction to TinkerCad

In this clinic, Fran will introduce TinkerCad. TinkerCad is an excellent CAD program for model railroaders. It is both easy to use, cloud based, and is well suited for the kinds of models and designs that model railroaders do. Fran will discuss the basics of how to get started and use the program.

Open Layout Locations

We will have 3 club and 7 member layouts open in the afternoon. This is a map of the approximate locations of the layouts. Addresses will be available at the SBRHS in the morning. This is information on the open layouts. Links below:

- [South Bay Railroad Historical Society](#)
- [California Central Model Railroad Club](#)
- [Silicon Valley Lines](#)
- [Dave Adams](#)
- [Robert Bowdidge](#)
- [Bob Brown, MMR](#)
- [Ed Loizeaux](#)
- [Dave Loveless](#)
- [Seth Neuman](#)
- [Ted Stephens](#)
- [Ray Turner](#)



Open Club Layouts

South Bay Railroad Historical Society

The South Bay Historical Railroad Society (SBHRS) was incorporated in 1985 to preserve the local area railroad heritage while running a working museum to benefit the community. Our members are volunteers and work on the renovation and preservation of the Santa Clara Railroad Depot, the Santa Clara Tower and the two maintenance-of-way buildings.

Within the Depot are two operational HO and N scale model railroad layout displays depicting various railroad eras and operations venues. Constant work by our members on these layouts provide an ever changing panorama, giving our visitors and members a better understanding of the different aspects of model railroading.



California Central Model Railroad Club

The California Central is an HO layout located in the old Agnews depot in San Jose. Scenically, the railroad is 95% complete. It is set in the 1950s, and locations are inspired by real locations from the California coast and SF Bay Area to Southern Oregon. There is also a dual gauge branch line; however, narrow gauge operations are infrequent. The California Central regularly conducts operations on the layout.



Silicon Valley Lines

Founded in 1979, Silicon Valley Lines (SVL) is an HO Scale Model Railroad Club located in San Jose, California. In 1999, we moved to our current location on 148 E. Virginia Street in San Jose, where our 23' x 72' layout is in operation. As one of a few model railroad clubs in the Bay Area, Silicon Valley Lines is dedicated to be an educational resource and support the model railroading community.

SVL operates trains using digital command control (DCC), which allows engineers to control locomotives independently on the same track. The Silicon Valley lines is an operations focused layout and has included remote operations. Members are given switchlist and must coordinate via radio with the dispatcher to obtain authority to move on the mainline and complete their work in a timely fashion. Our layout is constructed using some unique benchwork building techniques, with much of the layout supported by structural steel components.



Open Member Layouts

Dave Adams

D&RGW Durlin Branch

This railroad is a freelanced two headed branch line out of Chama NM on the D&RGW's fourth division narrow gauge line. Durlin is one of the branch terminals with industries representative of those in Silverton CO and Flint is the other branch terminal serving a coal mine based on the big CF&I breaker at Floresta CO. Durango and other points west of Chama are represented by staging, as is Alamosa and other points east of Chama. Prototype scenes include Chama, Cresco, Toltec Tunnel and Navajo.

An 18x27 room houses the layout which is a partial double deck design. Track is 100% complete, scenery is 96% complete and only one major structure remains to be built (meanwhile the foam core mock up works fine). 97% of the rolling stock is kit or scratch built, steam locomotives are primarily brass which have all been remotored/regeared/detailed/painted with primarily Soundtraxx decoders installed. Locomotive control is a CVP Easy DCC system with CVP wireless Ops throttles.

TT&TO, 4:1 fast clock, car cards and waybills along with a phone system for which telegraph sounders have replaced the phone bells support operations which are based on D&RGW practices in the 1920s. Uncoupling is 100% manual using pics. Water stops are augmented by both sound and animation, while other engine servicing events are timed by sound, as is the loading of stock cars.

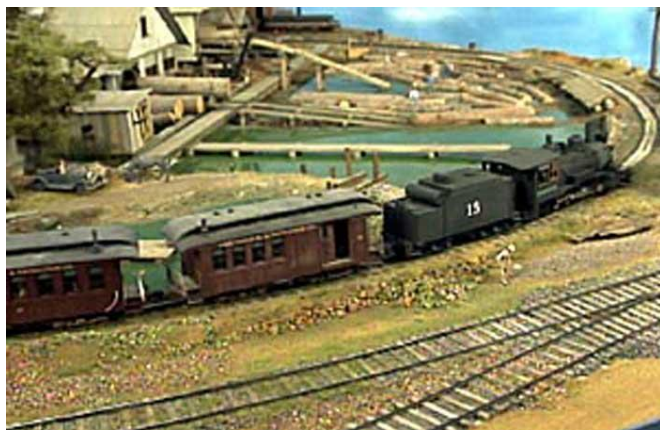


Bob Brown, MMR

Tuolumne Forks

Bob's "Tuolumne Forks" is a 28' x 28' On3 Sierra logging and mining RR, highly detailed and completely sceniced layout set in the Roaring Twenties. Included are 3 sawmills, a stamp mill, a resort hotel and lots of scratch-built structures. He recently added a Victorian O gauge section with a big hotel. Many of Bob's prize-winning models are incorporated into the layout.

Bob is editor and publisher of the Narrow Gauge & Short Line Gazette. Bob also has the On30/O standard gauge Shohorn Mining Co wharf and mine layout, an On30 Lime Kiln shelf layout, and a garden railway. There is also an extensive collection of models from the hobby's pioneers of 50-70 years ago. DCC.



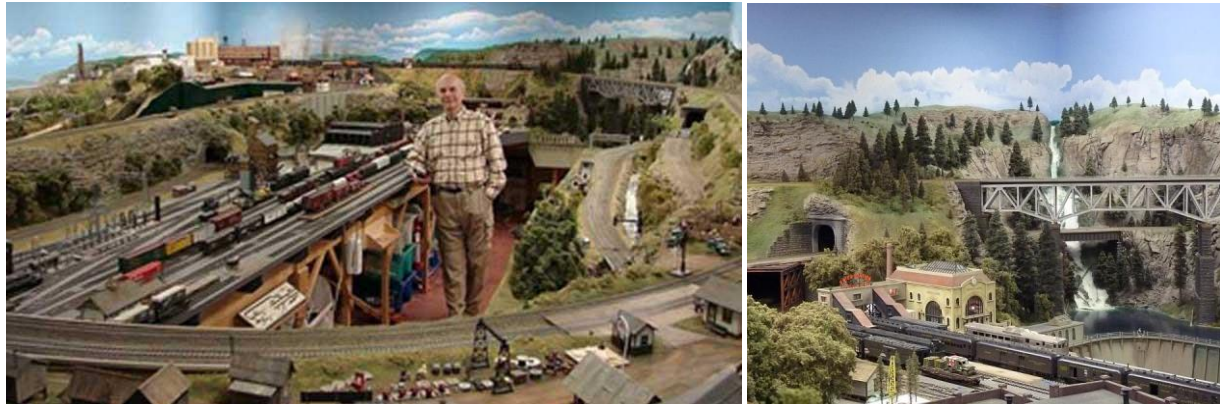
Ed Loizeaux

New York Central -- Valley Division

Pictures are worth many words. Click [here](http://sscale.org/best-of-s/ed-loizeaux/) for a video from the TRACKS AHEAD television show: <http://sscale.org/best-of-s/ed-loizeaux/>. Over 300 feet of double-tracked mainline with working DTC/ABS signaling in both directions, working semaphores, wireless control panels for the main yard, working turntable, computerized remote dispatching, a working 1950s-era telephone system and about 90 SwitchMaster-powered turnouts. Primary scenic features of this 20' x 30' layout include a 6' tall waterfall, a cut stone viaduct over 10' long, rain storm with flashing lightning and loud thunder, a large deck truss bridge, Mike Kotowski's professionally-painted backdrop, dozens and dozens of beautiful pine trees and billions of grains of ballast.

Control by wireless NCE DCC, CMR/I, JMRI, and DC.

This is NOT your father's American Flyer train set. It is genuine S scale model railroading. Visitors are encouraged to run a train while here. It's more fun per minute than watching TV! Caution: Crawlunder to enter layout room.



Dave Loveless

Rawhide & Duct Tape

The R&D railroad models the Southern Pacific Coast division in 1941 and is designed and built for operations. It resides in a purpose- built 24' X 36' building, with a double deck lift gate for easy egress. The railroad is double-decked, with the Southern Pacific Coast Division from Gilroy to Salinas on the lower deck and the Monterey Branch on the upper deck. The decks are connected by a single- track 5 level, custom built hidden helix that is equipped with sensors to indicate the location of a train.

The railroad has a 12 track staging yard. Each track is a through track, with the South representing East Salinas/King City/Los Angeles and the North end representing San Jose/ San Francisco /Roseville.

The layout currently has 100% of the track in place. 76 FastTrack turnouts have been built and installed. Scenery has just started but many scenes are mocked up with models and temporary structures.

The railroad is NCE DCC controlled with wireless throttles, and hand thrown turnouts. Dispatching is done using 1. Track Warrants via FRS radios or 2. TT/TO via telephones. Concise documentation of all train operations is provided to assist operators in stepping into a new job.



Seth Neumann

Union Pacific Niles Canyon, HO

The layout is set in the East Bay Area in 1999. The railroad represents the former Western Pacific 1st and San Jose Subdivisions, which had become the UP Niles, Oakland and Milpitas Subdivisions in the era modeled. The railroad is a secondary main and a branchline which supports an Auto plant (NUMMI) and a number of smaller industries.

The layout is built in a 450 sqft purpose built room. Construction is single level. Dispatching is by TCS (using CATS) which replicates a Digicon console. Signal control is by CMRInet, including cpNodes made by Seth's company, Model Railroad Control Systems. All signals are repeated as color lite signals on repeaters above the layout. There is a complete writeup of the layout in Volume 52 of the Layout Design Journal, publication of the Layout Design SIG and a cover story in RMC January 2018. The layout is about 95% sceniced.

A unique feature of the layout is that there is no yardmaster, only a Clerk. This is because the prototype Milpitas Yard has no permanent switcher. The Clerk is assisted by a working RFID reader which prints "track lists" of cars in order as a switch job pulls cuts over the reader. The RFID system appeared in Modeler Railroader, December 2014, page 46. The clerk is responsible for managing yard and the neighboring NUMMI complex by writing switchlists to instruct the crews where to place cars. All crews do their own work when passing through the yard.



Ted Stephens

Ohio and Little Kanawha

The Ohio & Little Kanawha Railroad is a freelance 1950s West Virginia coal hauling railroad. It travels around West Virginia, not through. The Ohio & Little Kanawha shares mainline and a Yard with the Chesapeake & Ohio. The railroad features 220 feet of single and double track mainline that is mostly hand laid track. All turnouts are scratch built using Fast Tracks assembly fixtures. The scenery is about 85% complete. The layout features many structures that include scratch built and craftsman type kits. The layout is built in a 750 square foot room of an industrial building. The main layout room is 50 x 15 with a branch in an adjacent room. Construction is single level. The layout features dozens of exquisite craftsman structures.

Operation and Control:

- NCE-DCC with wireless throttles
- Car forwarding by car cards and waybills
- Dispatching via CTC with trackside telephones
- Manual uncoupling using picks
- All mainline engines are equipped with sound decoders
- 2:1 fast clock



Ray Turner

Mystic Mountain Railroad

The Mystic Mountain Railroad is a back country shortline loosely set in the early 1900's. The MMRR has 1200' of track, two yards, two wyes for turning trains, a helix inside a mountain, many tunnels and bridges, and 27 industry spots for operations. The Mountain Division runs over rugged mountains and deep canyons, necessitating several steel bridges. Trains are run point-to-point between the two yards for the operating sessions held monthly in the summer. Trains are battery-powered radio controlled with sound. JMRI is used for car movement scheduling.

Construction of the MMRR took about 10 years with considerable labor in the concrete rock castings.

Custom-built structures include:

- the roundhouse, turntable, station, and coal/water tower at North End yard,
- the ore processing plant (120 lb. cast concrete) at Outaluck,
- the ACME Manufacturing factory (Acrylic),
- the wharf and Mercury Canning Co. (Acrylic, PVC) with sound effects,
- Hazel's Hardware (old OSH box car with interior details & lighting)
- the Sawmill complex (Acrylic, metal, recycled political signs) with sound effects.

In 2014 changes were made to improved railroad operations adding several industrial spurs. In 2018 the new town of Costa was added with six new industries (a switching puzzle).

